

# G2T OPEN WEB TRUSS

## JOB SITE HANDLING OF G2T OPEN WEB TRUSSES

IT IS THE BUILDING CONTRACTOR'S RESPONSIBILITY TO UNLOAD THE G2T TRUSSES FROM THE TRUCK AND FOR ALL HANDLING THEREAFTER. THE G2T OPEN WEB TRUSS GUARANTEE ONLY APPLIES AS LONG AS THE PRODUCT IS NOT DAMAGED OR ALTERED IN ANY WAY. IT IS INSTALLED IN A WORKMANLIKE MANNER AND ACCORDING TO THE INSTALLATION INFORMATION NOTED HEREIN. G2T TRUSSES WILL BE DELIVERED TO THE JOBSITE IN BUNDLES Banded TOGETHER FOR HANDLING EASE. TO AVOID DAMAGE, TRUSSES SHOULD BE LEFT IN THESE BUNDLES UNTIL READY FOR INSTALLATION IN THE STRUCTURE. A CARELESS CRANE OR FORKlift OPERATOR CAN DAMAGE G2T TRUSSES. NEVER HANDLE G2T TRUSSES FLAT - KEEP IN AN UPRIGHT POSITION.

### STORAGE OF G2T OPEN WEB TRUSSES

DURING STORAGE AT THE JOBSITE, KEEP G2T TRUSSES IN AN UPRIGHT POSITION. THE BUNDLES SHOULD BE SUPPORTED ON LEVEL STICKERS TO KEEP THE G2T TRUSSES OUT OF THE MUD AND DIRT. STACKING OF BUNDLES IS PERMITTED IF AN ADEQUATE NUMBER OF STICKERS ARE PROVIDED TO PREVENT DAMAGE AND NORMAL SAFETY PRECAUTIONS ARE FOLLOWED. ALL GLUE USED IN G2T TRUSSES IS WATER PROOF. HOWEVER, LONG EXPOSURE TO WATER AND SUN WILL CAUSE SOME DETERIORATION AND CHECKING OF WOOD. G2T TRUSSES SHOULD RECEIVE THE SAME PROTECTION FROM WEATHER AS OTHER WOOD PRODUCTS.

### TYPICAL G2T PROJECT NOTES:

- FOR NOTES, DETAILS, AND DIMENSIONS NOT ON THESE PRODUCT OR CEMENT PLAN DRAWINGS, REFER TO PROJECT PLANS.
- SEE BILLS OF MATERIAL FOR ITEMS FURNISHED.
- ALL CLOUDED NOTES, DIMENSIONS, ETC. REQUIRE VERIFICATION AND MUST BE MARKED ON THE CORRECT INFORMATION PROVIDED BY CUSTOMER, PRIOR TO RETURN TO BEING RETURNED FOR FABRICATION.
- PLEASE BE AWARE THAT ANY CLOUDED ITEMS NOT ACKNOWLEDGED WILL REQUIRE CONTACT WITH RESPONSIBLE PARTIES AND MAY CAUSE DELAY IN THE PROCESSING OF YOUR ORDER.
- PLEASE VERIFY THAT ALL INFORMATION PROVIDED HEREWITH REFLECTS THE LATEST AVAILABLE PROJECT INFORMATION AND THAT ALL G2T TRUSS LENGTHS CORRESPOND WITH ACTUAL FIELD DIMENSIONS PRIOR TO BEING RETURNED FOR FABRICATION.
- ALL BRACING SHOWN IS INTEGRAL TO THE G2T OPEN WEB TRUSS SYSTEM AND IS NOT TEMPORARY OR ERECTION BRACING. THE PCT OPEN WEB TRUSS WILL NOT SAFELY SUPPORT LOADS UNTIL FULLY BRACED, FULLY ATTACHED TO BEARING WALLS OR BEAMS, AND SHEATHING, IS PROPERLY INSTALLED (SEE LAYOUTS AND DETAILS).
- POINT LOADS THAT EXCEED 100 LBS. AS INDICATED ON THE PLACEMENT PLAN HEREIN.
- INSTALLATION OF G2T OPEN WEB TRUSSES MUST FOLLOW ANY ADDITIONAL REQUIREMENTS INDICATED ON THE PLACEMENT PLAN AND IN THE CALCULATIONS.
- ALL G2T OPEN WEB TRUSSES ARE DESIGNED FOR UNIFORM LOADS AND CONCENTRATED LOADS NOTED ON THESE DRAWINGS AND CALCULATIONS. TEMPORARY CONSTRUCTION LOADS WHICH CAUSE STRESSES BEYOND DESIGN CRITERIA ARE NOT PERMITTED.
- ALL 2X, 4X, 6X ETC. FRAMING TO BE SUPPLIED BY OTHERS, UNO. (FMBO).
- METAL STRAPS AND/OR TIES USED FOR SEISMIC PURPOSES THAT ARE NAILED TO THE TOP OF THE TOP CHORD ARE TO USE 10d NAILING AT NO LESS THAN 3" ON IN A ROW UNO ACCEPTABLE STRAPS FOR G2T TOP CHORDS ARE LTI, LSTI, MSTI AND PAI OR OTHER COMPARABLE ITEMS UNO.
- G2T OPEN WEB TRUSS ARE NOT DESIGNED TO SUPPORT ANY FIRE SPRINKLER AND/OR MECHANICAL LOADS OTHER THAN WHAT IS SHOWN WITHIN THESE DRAWINGS, AND OR WHAT HAS BEEN PROVIDED IN THE DESIGN DEAD LOAD(S).
- THE PLACEMENT OF THE MECHANICAL UNITS AND SPRINKLER MAINS ARE TO BE AS NOTED ON THE PLACEMENT PLAN. THE SUPPORTING TRUSSES HAVE BEEN SPECIFICALLY DESIGNED TO ACCOMMODATE THESE ITEMS. ALL COMPONENTS TRANSFERRING LOADS TO THE TRUSSES SHALL BE DESIGNED AND INSTALLED IN ACCORDANCE WITH THE DETAILS CONTAINED WITHIN THESE DRAWINGS. IF THE ACTUAL LOCATIONS DO NOT COINCIDE WITH THESE DRAWINGS CONTACT THE MANUFACTURER.
- G2T TRUSS DESIGNS ARE IN ACCORDANCE WITH THE CURRENT ADOPTED EDITION OF THE IBC, CBC, AND NATIONAL DESIGN SPECIFICATION, AND CONFORM TO CURRENT ICC-ES REPORT.

REVISIONS

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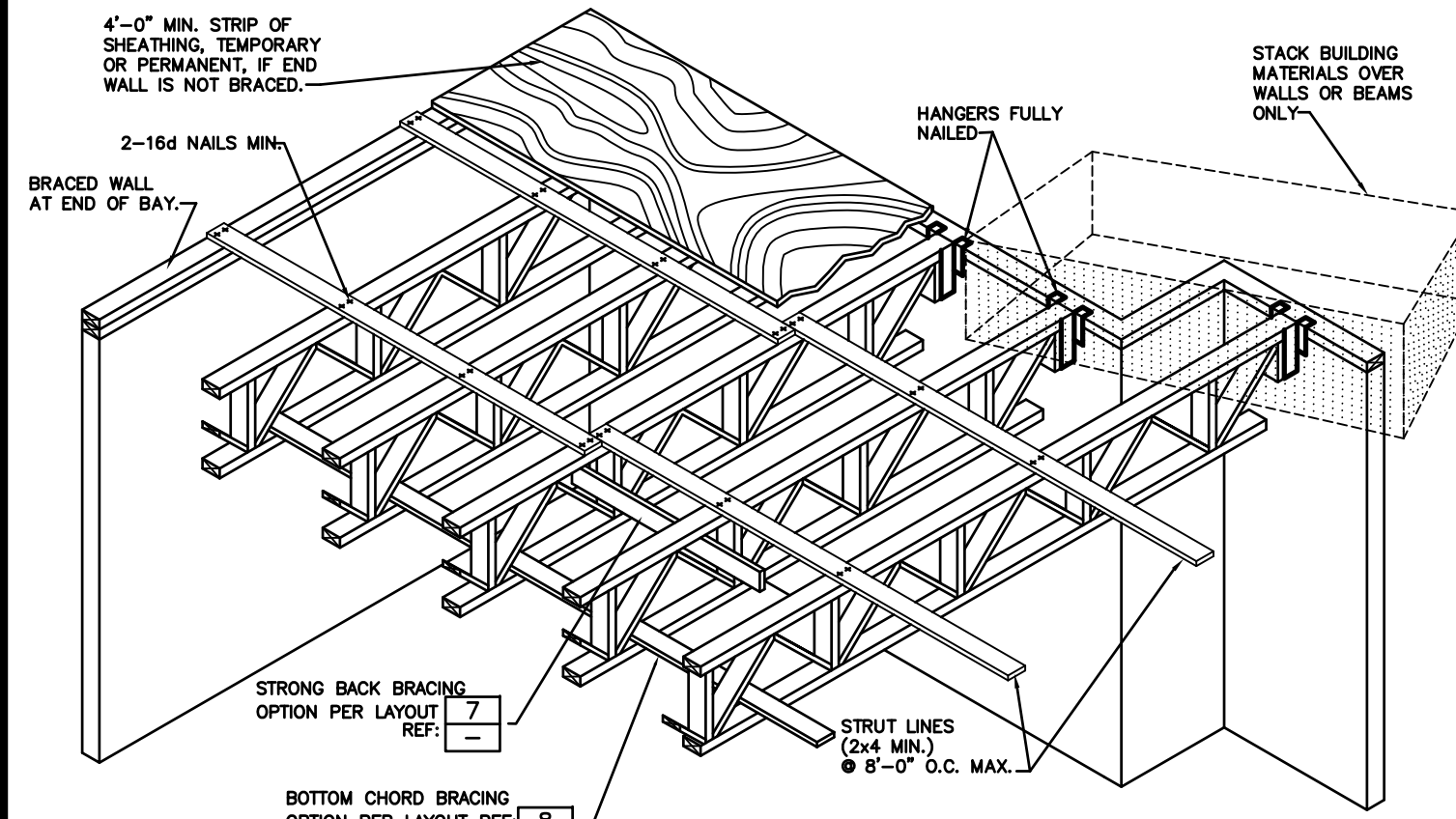
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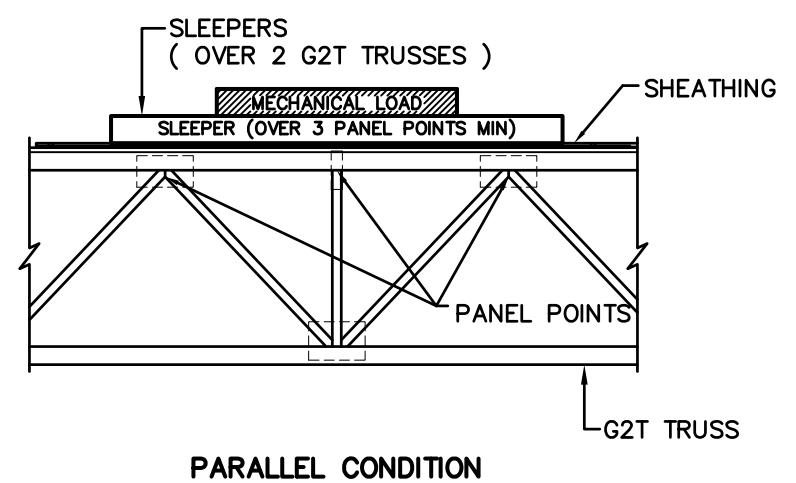
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SHEET  
1 OF 3

## ATTENTION

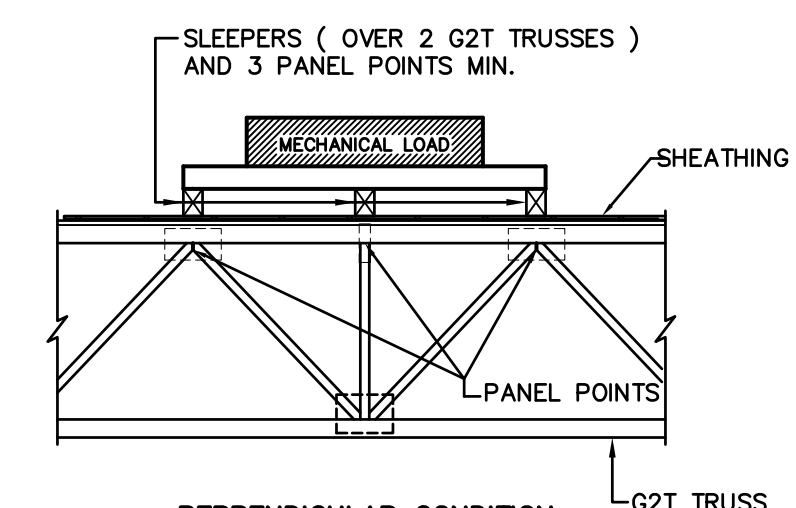
NO ONE SHOULD BE ALLOWED ON ANY G2T JOIST UNTIL ALL HANGERS & CROSS BRIDGING, IF REQ'D, AND TEMPORARY BRACING ARE IN PLACE AND NAILED SECURELY. SERIOUS ACCIDENTS MAY OCCUR UNLESS CARE IS TAKEN TO PROPERLY BRACE DURING CONSTRUCTION. THIS DETAIL SHOULD BE USED AS A GUIDELINE FOR BRACING.



## 1 ERECTION BRACING



PARALLEL CONDITION



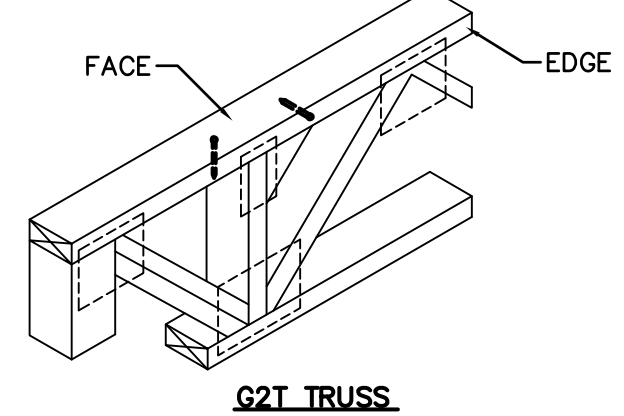
PERPENDICULAR CONDITION

- NOTES**
- COORDINATE MECHANICAL LOCATIONS WITH LAYOUT AND G2T CALCULATIONS
  - SLEEPERS MUST BE LOCATED AT PANEL POINTS

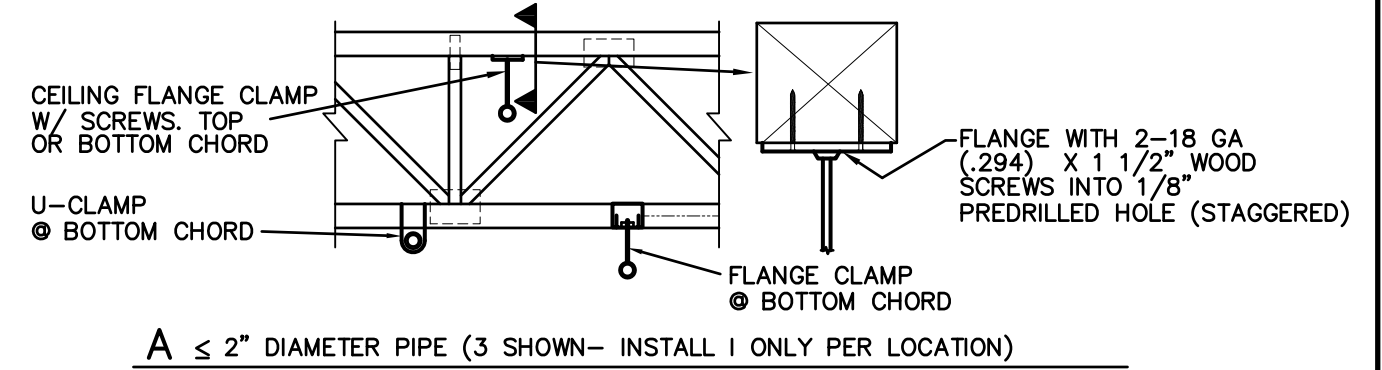
## 2 MECHANICAL LOADS ON G2T TRUSSES

G2T TRUSS NAILING CHART  
(MINIMUM ON CENTER SPACING)

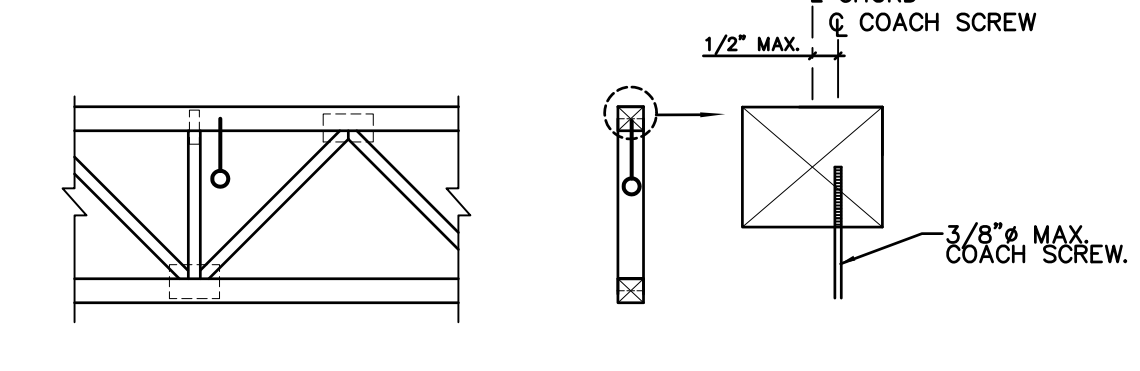
NAIL TYPE	NAIL SIZE	MSR (M)	LSL (L)		LVL (V)	
			FACE	EDGE	FACE	EDGE
8d	BOX	0.113"x2 1/2"	2"	2"	3"	3"
	COMMON	0.131"x2 1/2"	2"	2"	3"	3"
10d	BOX	0.128"x3"	2"	2"	3"	3"
	COMMON	0.148"x3"	3"	4"	3"	4"
12d	BOX	0.128"x3 1/4"	3"	2"	3"	3"
	COMMON	0.148"x3 1/4"	3"	4"	3"	4"
16d	BOX	0.135"x3 1/2"	3"	3"	3"	4"
	COMMON	0.148"x3 1/4"	3"	4"	3"	6"
			4"	6"	3"	6"



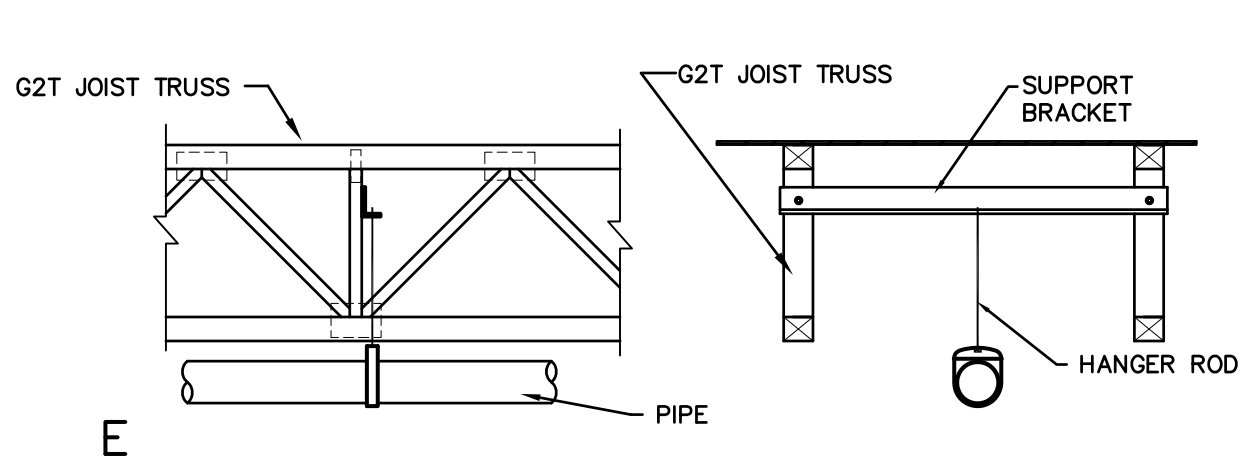
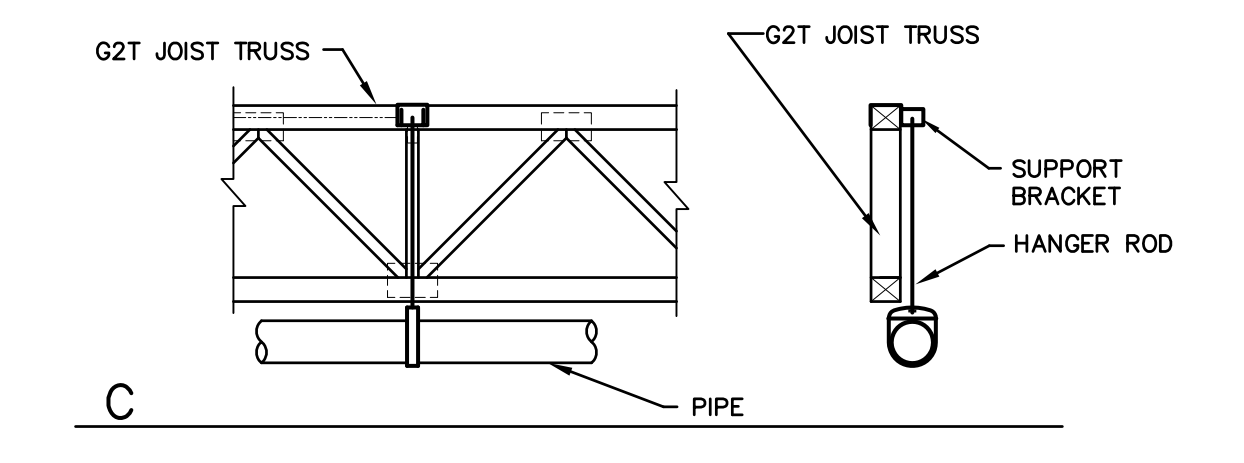
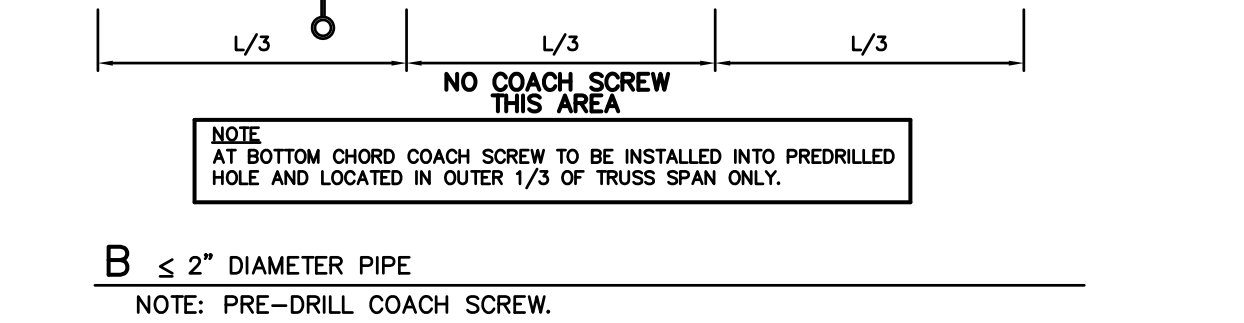
## 3 NAILING CHART



A ≤ 2" DIAMETER PIPE (3 SHOWN- INSTALL 1 ONLY PER LOCATION)

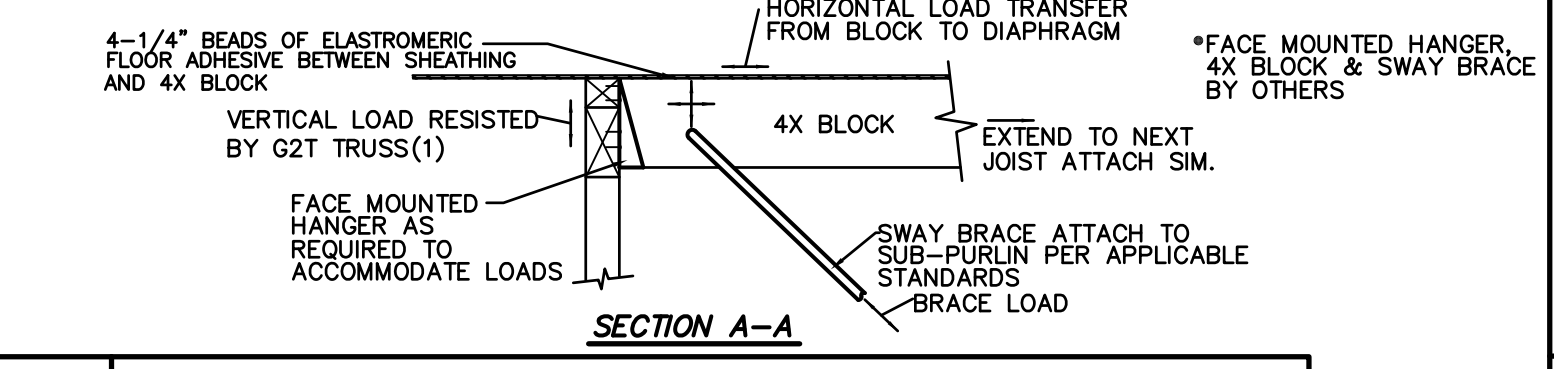
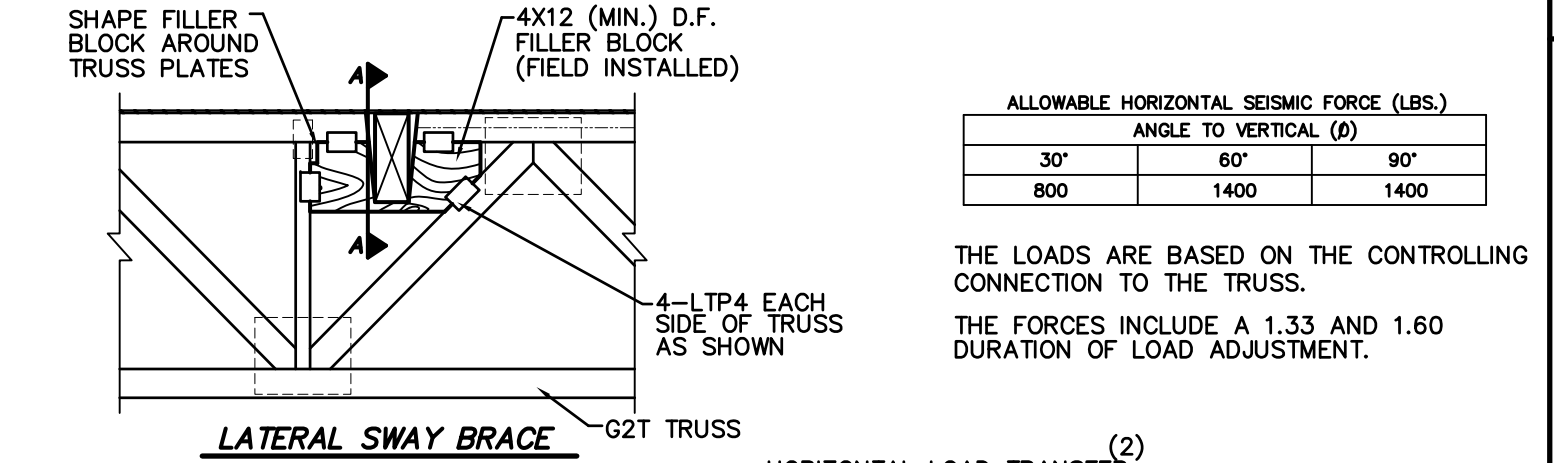
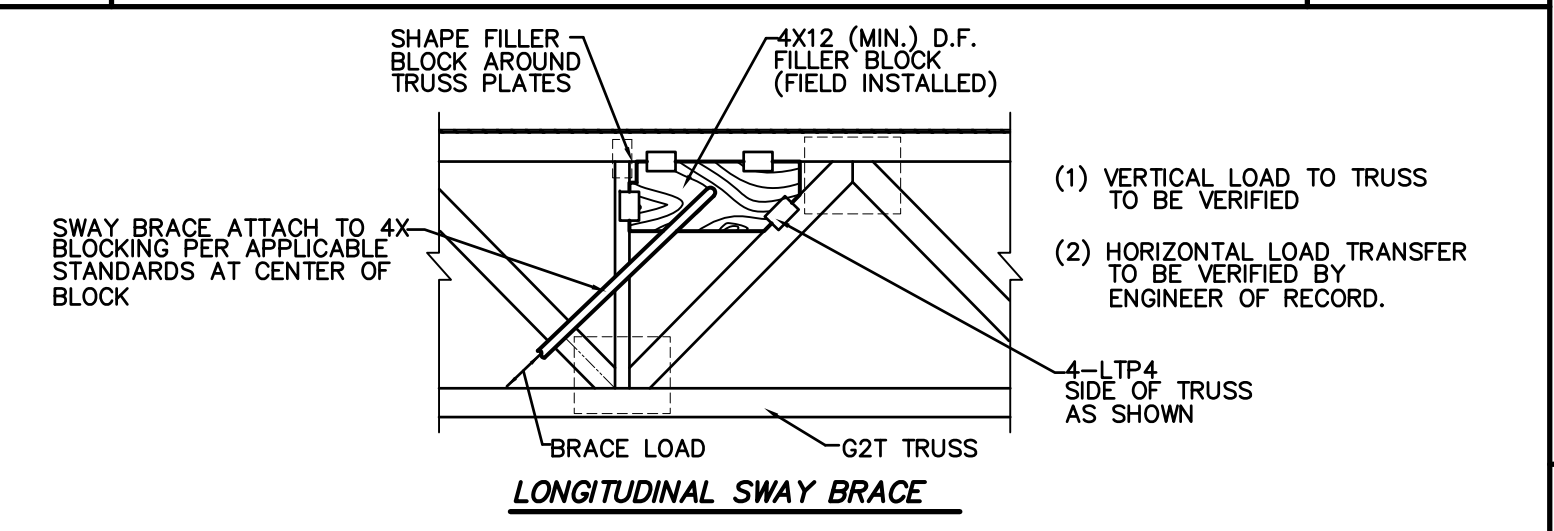


B ≤ 2" DIAMETER PIPE  
NOTE: PRE-DRILL COACH SCREW.



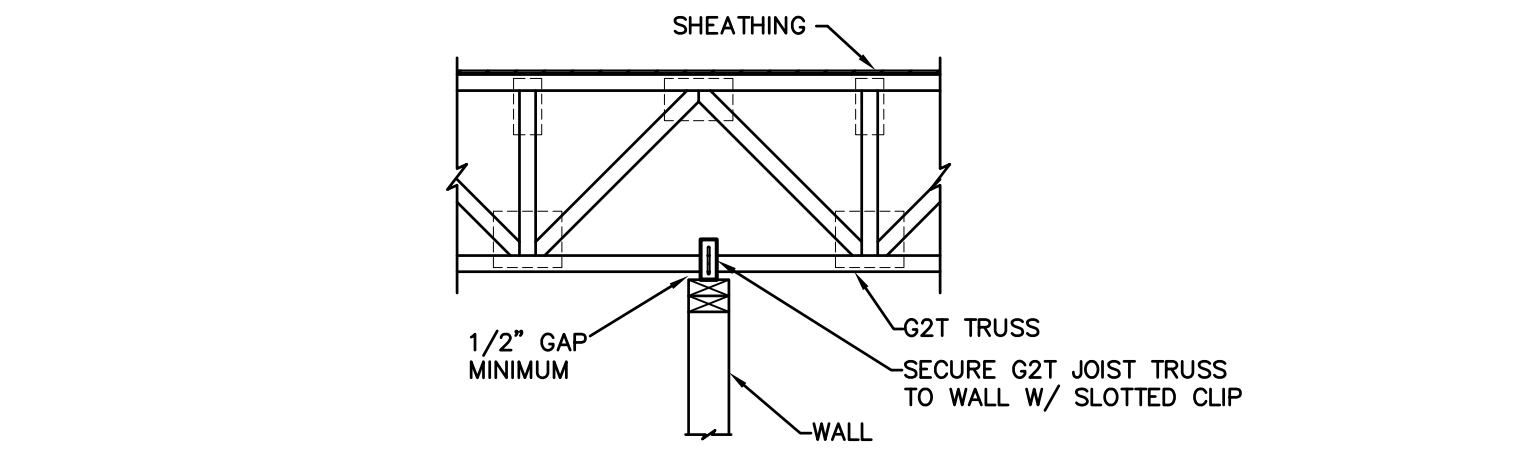
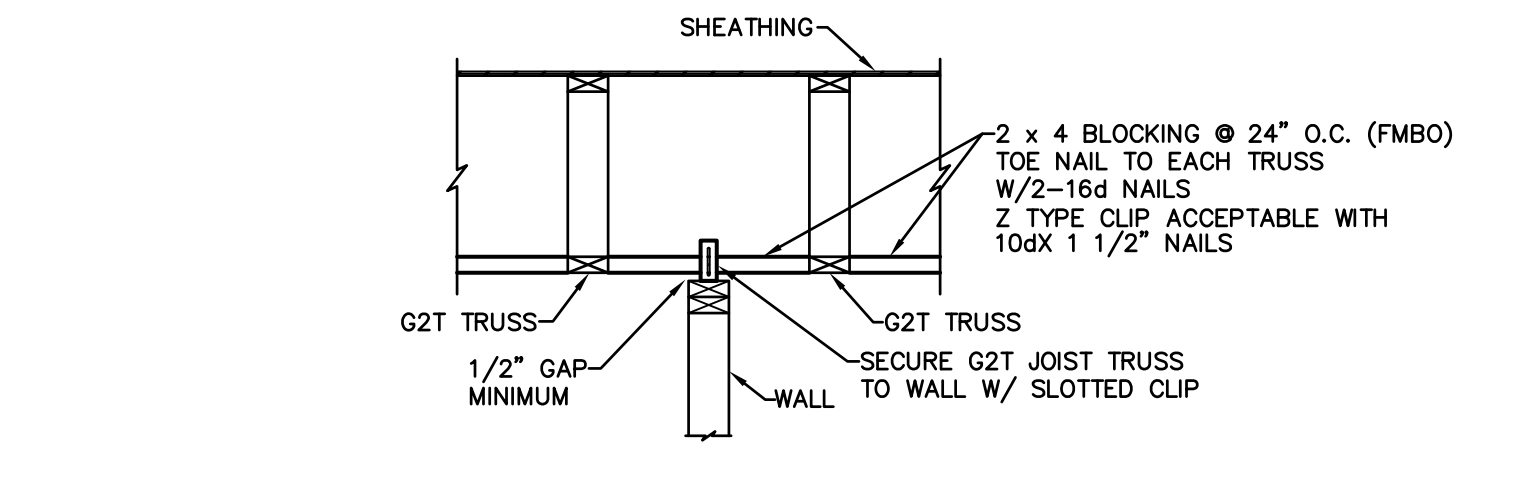
- NOTES:**
- WOOD SCREWS WITH A MAXIMUM DIAMETER OF 1/8" MAY BE USED ANYWHERE ON THE TOP CHORD OF THE PCT TRUSS WITH A PRE-DRILLED PILOT HOLE, UNLESS NOTED OTHERWISE ON PLANS.
  - DO NOT DRILL HOLES, DRIVE HEAVY SCREWS, OR USE LAG BOLTS IN THE BOTTOM CHORD OF G2T TRUSS, EXCEPT AS NOTED.
  - COORDINATE ATTACHMENT OF SPRINKLER PIPE GREATER THAN 3" DIAMETER AND LARGER WITH G2T TRUSS LAYOUT AND CALCULATIONS. IT IS ASSUMED THAT THE ENGINEER OF RECORD HAS ALLOWED FOR BRANCH SPRINKLER LINES 3" DIAMETER AND LESS IN THE DESIGN DEAD LOAD.
  - ALL SPRINKLER AND SPRINKLER ATTACHMENTS INCLUDING WOOD SUPPORTS ARE TO BE FURNISHED AND INSTALLED BY OTHERS.

## 4 FIRE SPRINKLER ATTACHMENT DETAIL

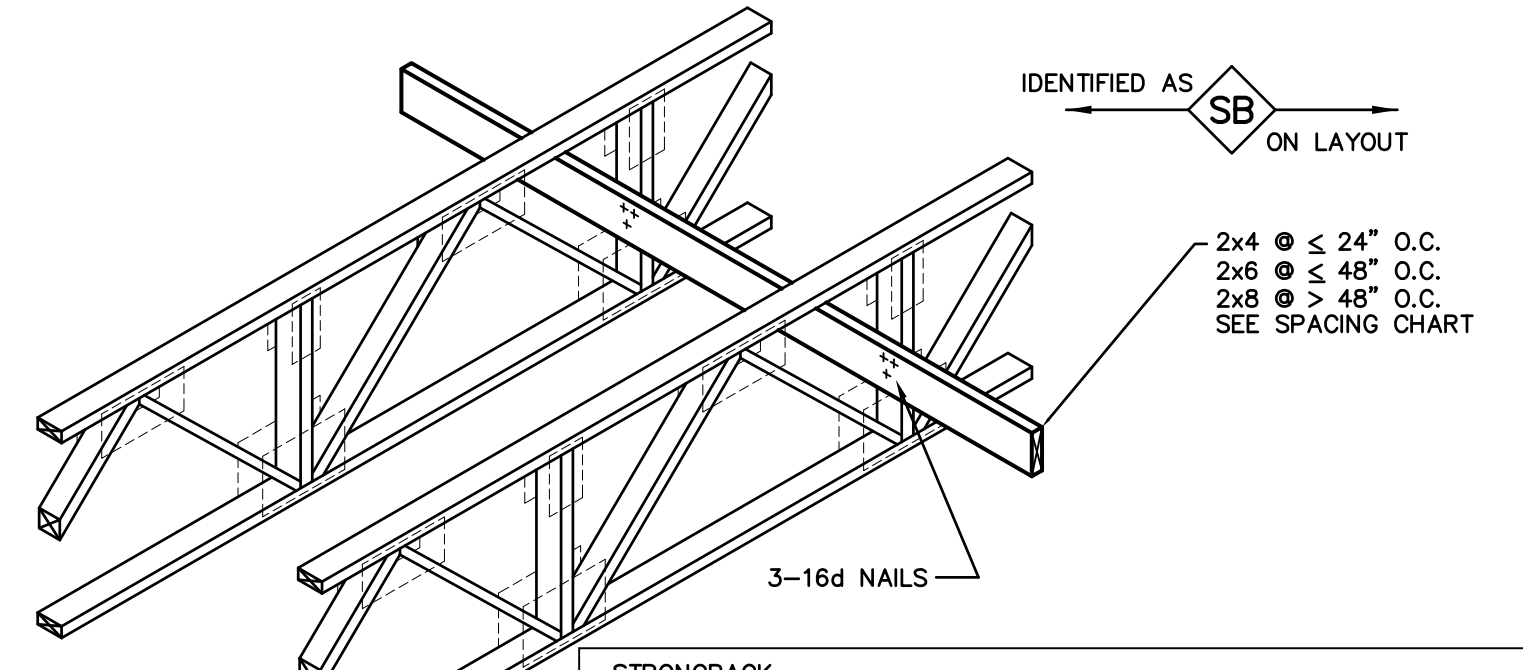


## 5A FIRE SPRINKLER SWAY BRACE DETAIL

## 5B FIRE SPRINKLER SWAY BRACE DETAIL



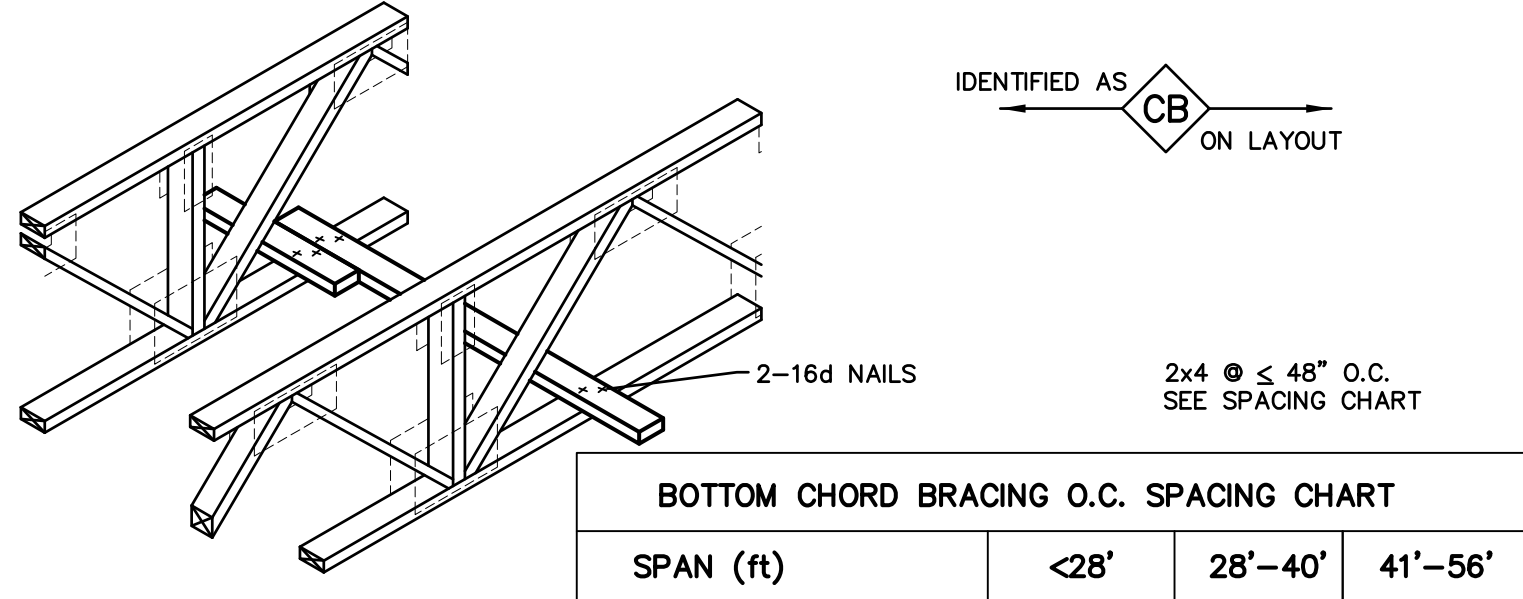
## 6 TRUSS AT NON-BEARING WALL



STRONGBACK DF or SPF #2	STRONGBACK O.C. SPACING CHART		
SPAN (ft)	<28'	28'-40'	41'-56'
# OF ROWS ROOF	1	2	3
# OF ROWS FLOOR	1	2	N/A

MAXIMUM ON CENTER SPACING OF BRIDGING IS 14'-0"  
THIS BRACING MATERIAL IS INTEGRAL TO THE PERFORMANCE OF THE TRUSS AND IS TO BE CONSIDERED PERMANENT.

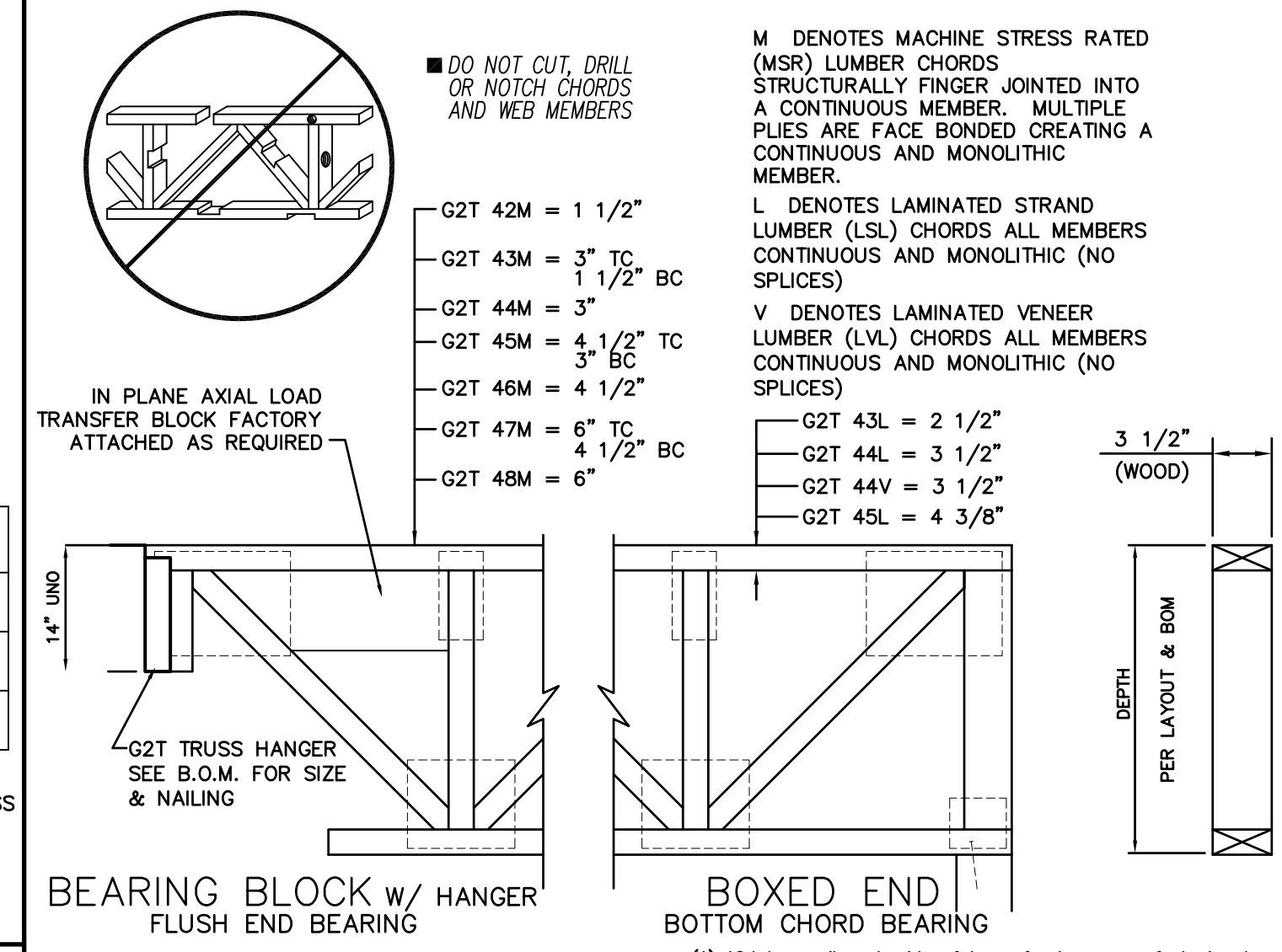
## 7 STRONGBACK DETAIL



BOTTOM CHORD BRACING O.C. SPACING CHART			
SPAN (ft)	<28'	28'-40'	41'-56'
# OF ROWS ROOF	1	2	3

MAXIMUM ON CENTER SPACING OF BRIDGING IS 14'-0"  
THIS BRACING MATERIAL IS INTEGRAL TO THE PERFORMANCE OF THE TRUSS AND IS TO BE CONSIDERED PERMANENT.

## 8 BOTTOM CHORD BRACING DETAIL



## 8 G2T TRUSS NOTES AND PROFILES

- LEGEND / ABBREVIATIONS**
- SEE PROJECT PLANS FOR OTHER ABBREVIATIONS AND SYMBOLS USED.
- DETAIL (ON SHOP DRAWINGS)
  - PROJECT PLAN DETAIL (PER PLANS)
  - START G2T TRUSS LAYOUT @ o/c SPACING
  - STRONGBACK LOCATION
  - DIRECTION OF ROOF SLOPE
- FMBO = FRAMING MATERIAL BY OTHERS**
- VF = VERIFY IN FIELD  
NIC = NOT IN CONTRACT  
UNO = UNLESS NOTED OTHERWISE  
FSML = FIRE SPRINKLER MAIN LINE  
FIT = FACE TO FACE (CLEAR SPAN OF TRUSS)  
MTL = MANUFACTURED TRUSS LENGTH  
OTCL = OVERALL TOP CHORD LENGTH (SLOPE LENGTH)  
LBS = POUNDS  
PSF = POUNDS PER SQUARE FOOT  
PLF = POUNDS PER LINEAL FOOT  
MFR. = MANUFACTURER  
> = GREATER THAN (< = LESS THAN)  
o/c, o.c. = ON CENTER (SPACING)  
BOM = BILL(S) OF MATERIAL  
DBL = DOUBLE MEMBER (TPL = TRIPLE MEMBER)



G2T TRUSS LIST						
MARK	QTY	CLR SPAN	MFR LENGTH	DEPTH	HANGER (BLACK END)	HANGER (OTHER END)
RT01	144	36'-1 1/2"	36'-0 3/4"	33" G2T-43M	RAU413	RAU413

KC METALS CONNECTORS ESR-2930					
QTY	MARK	DESCRIPTION	NAILING		REMARKS
			HEADER	JOIST	
288	RH01	RAU413, H=14" W=3 5/8"	6-16d	6-10d x 1 1/2	SINGLE

MISC. MATERIAL LIST			
ITEM	QTY	REMARKS	
2x6 S/B BRACING	1193 LF	STD G2T S/B BRACING	(SEE DETAIL 7/1)

PROJECT PLANS USED:	
SHEETS	DATE
ARCHITECTURAL DRAWINGS CV THRU A-4.0	06/11/15
STRUCTURAL DRAWINGS S-1 & SD-1	08/15/17
MECHANICAL DRAWINGS MP0.1 THRU MP1.1	07/02/15

DESIGN LOADS	
	ROOF
LIVE LOAD	20 PSF
PART LOAD	- PSF
DEAD LOAD	18 PSF
TOTAL LOAD	38 PSF

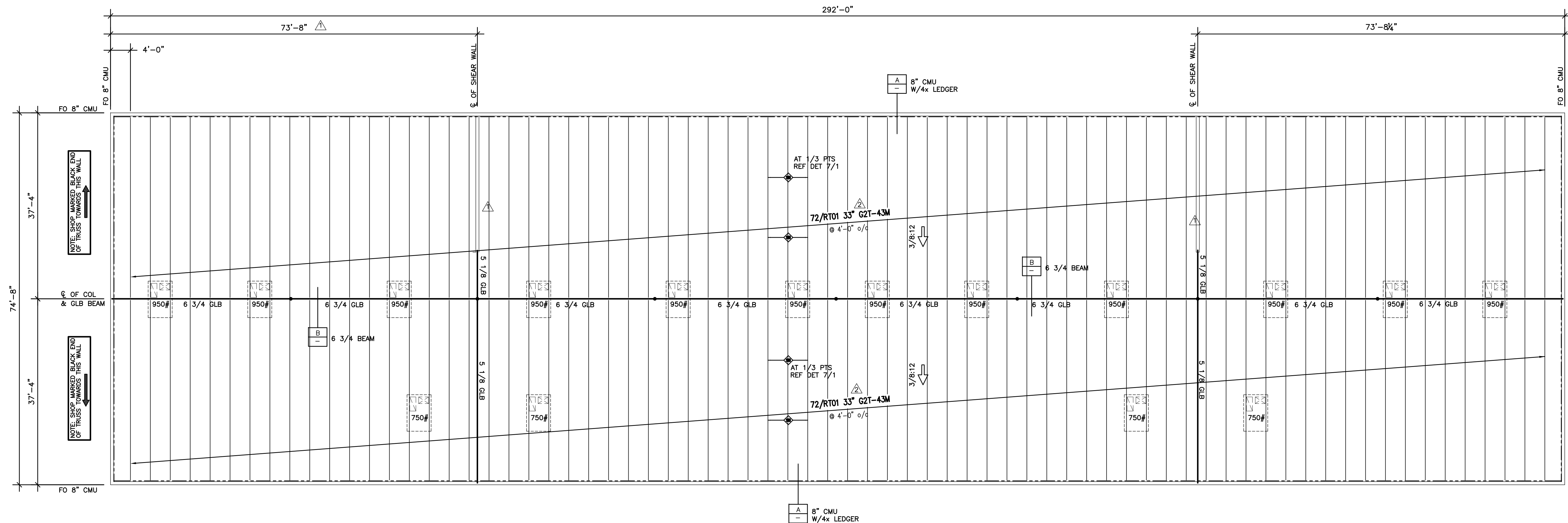
DESIGN DEFLECTION:  
 $\Delta_{LL} \leq L/360$   $\Delta_{TL} \leq L/240$

LOAD DURATION FACTOR = 1.25  
 DESIGN CODE: IBC 2012  
 DEAD LOAD BREAKDOWN: 14 PSF TC  
 4 PSF BC  
 115 MPH WIND, EXP. B

- ROOF LIVE LOAD REDUCIBLE PER CODE.  
 - ADDITIONAL 250# MISC POINT LOAD OCCURRING AT ANY ONE TOP CHORD PANEL POINT ALONG THE SPAN, CONCURRENT WITH UNIFORM DEAD LOAD AND LIVE LOAD ONLY.  
 - AXIAL LOAD: 2600 LBS MAX

NOTE: SPRINKLER LINES GREATER THAN 3" DIAMETER HAVE NOT BEEN CONSIDERED IN THE TRUSS DESIGNS

NOTE: SKYLIGHT LOCATIONS TO FIT BETWEEN TYPICAL JOIST SPACINGS



ROOF PRODUCT PLACEMENT PLAN

(REF: S-1)  
 SCALE: 3/32" = 1'-0"

REVISIONS
△ B-28-17 Per revised S-1 & SD-1
△ B-31-17
△
△
△

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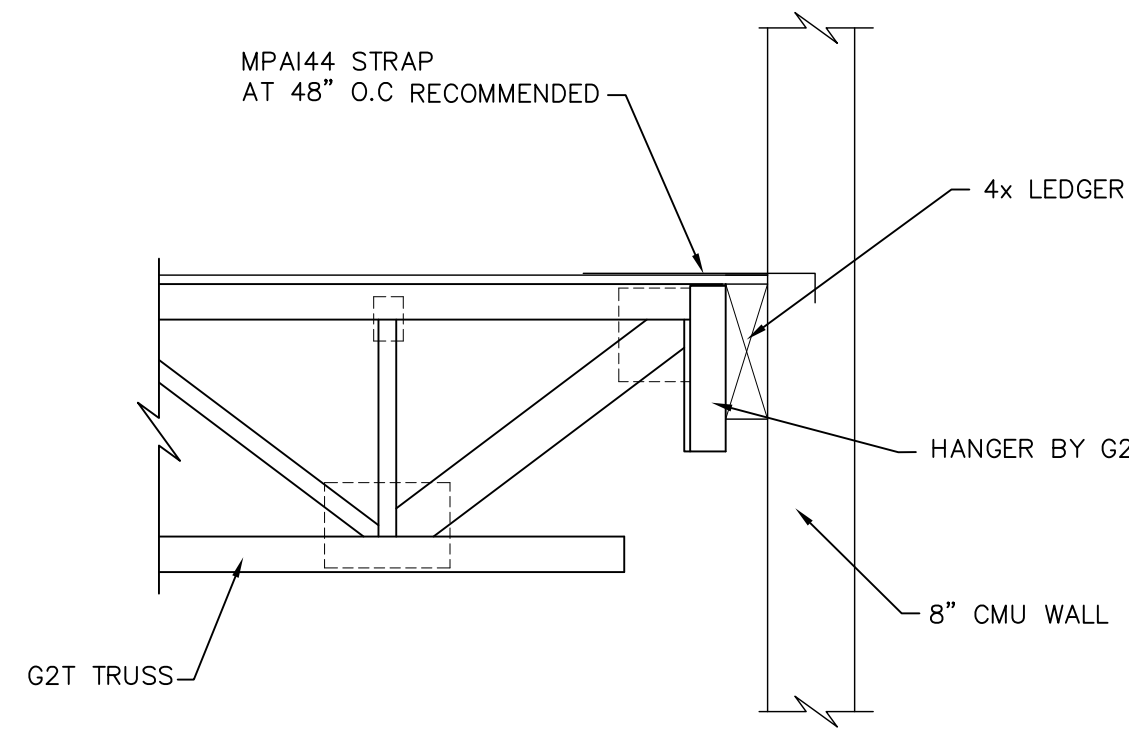
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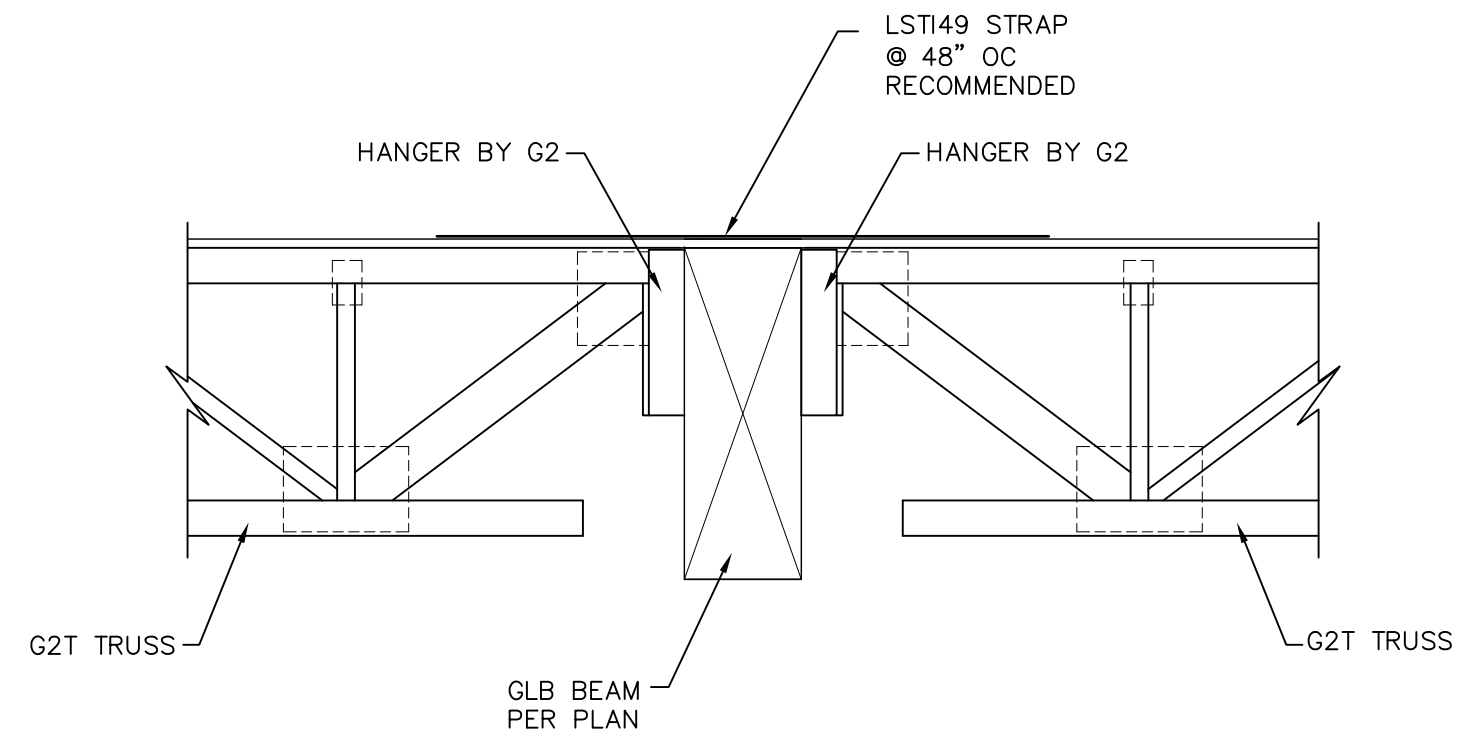
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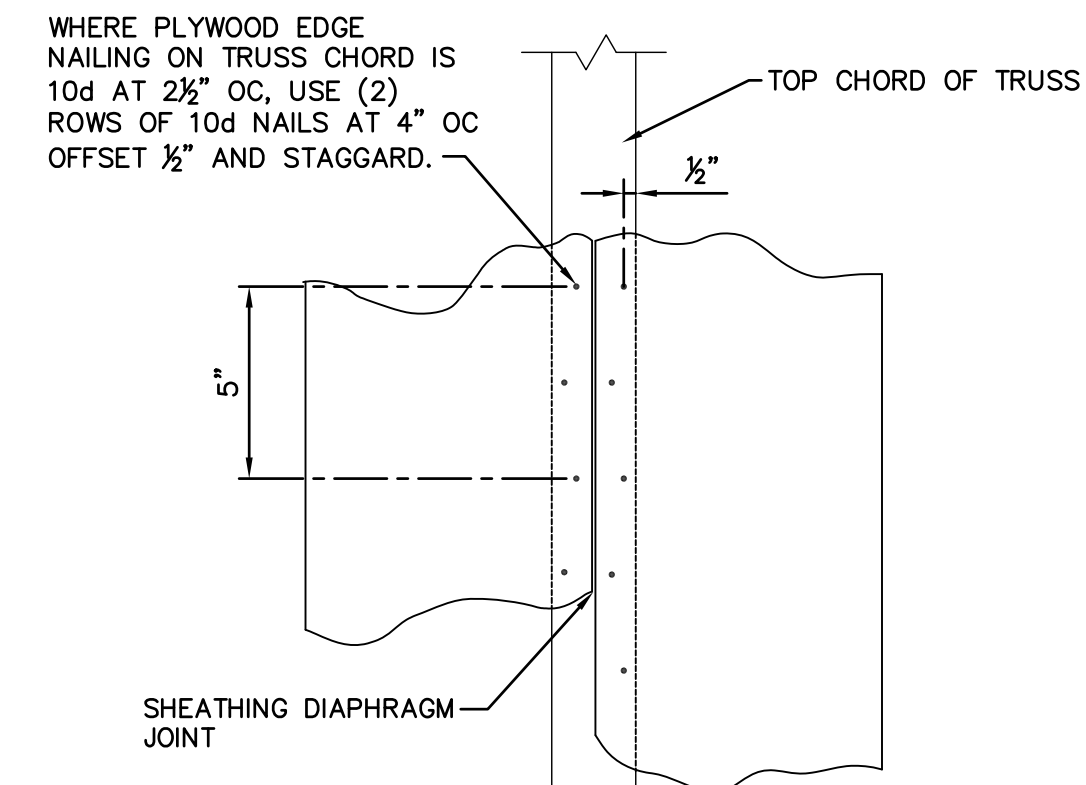
REF: L/SD-1

**A**  
-



REF: 1/S-1

**B**  
-



PLYWOOD NAILING DIAGRAM  
(@ 2 1/2" OC)

**C**  
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REVISIONS	
△	B-208-17 Per revised S-1 & SD-1
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△	.
△	.
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3 OF 3

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